

HCRH Segments



opportunity to construct a connection project, because there is little available land between the Columbia River, the Union Pacific Railroad, Interstate 84 and an unstable, steep hill.

Vision – The Herman Creek Road over Wyeth Bench, while steep, is paved and should be used as the hiking and biking connection between Cascade Locks and Wyeth.

g) Wyeth to Starvation Creek

Old mile post 52.8–57

Existing Condition – This section includes Shellrock Mountain. The option to provide a hiking or biking trail behind the binwall (in the rockfall catch area) was a low priority in the 1987 Study. There is a segment of pavement north of the Union



Figure 27—Moss covers the Original Highway Surface near Lindsey Creek

Pacific Railroad that would be desirable to connect to other segments.

Vision – Construct HCRH State Trail between Wyeth and Starvation Creek. An option that is north of the Union Pacific Railroad from Wyeth to east of Shellrock Mountain, crossing over Interstate 84 to the Lindsey Creek State Park segment is proposed. This proposal includes a mile-long floating path in the Columbia River, similar to the Vera Katz Eastbank Esplanade in Portland.

h) Starvation Creek to Viento

Old mile post 57–58.2

Existing Condition – The Starvation Creek to Viento connection project has linked the quiet waterfall on Starvation Creek with the views up and down the Columbia, and the camping area of Viento State Park, with access to the river. Interpretive signs have recently been added at Starvation Creek and within Viento State Park.

Vision – Additional funding is needed to decrease the grade and pave the Viento end of this segment, to make it wheelchair accessible, if possible.

i) Viento to Mitchell Point

Old mile post 58.2–61.1

Existing Condition – The Wygant Trail connects some portions of HCRH pavement. The 1987 Study recommended reopening the portion of the Wygant Trail along Perham Creek and extending the trail to Viento State Park.

Vision – Construct the HCRH State Trail between Viento and Mitchell Point. The September 2000 scoping team recommended a line south of Interstate 84, including several rockfall hazard areas that need additional analysis.

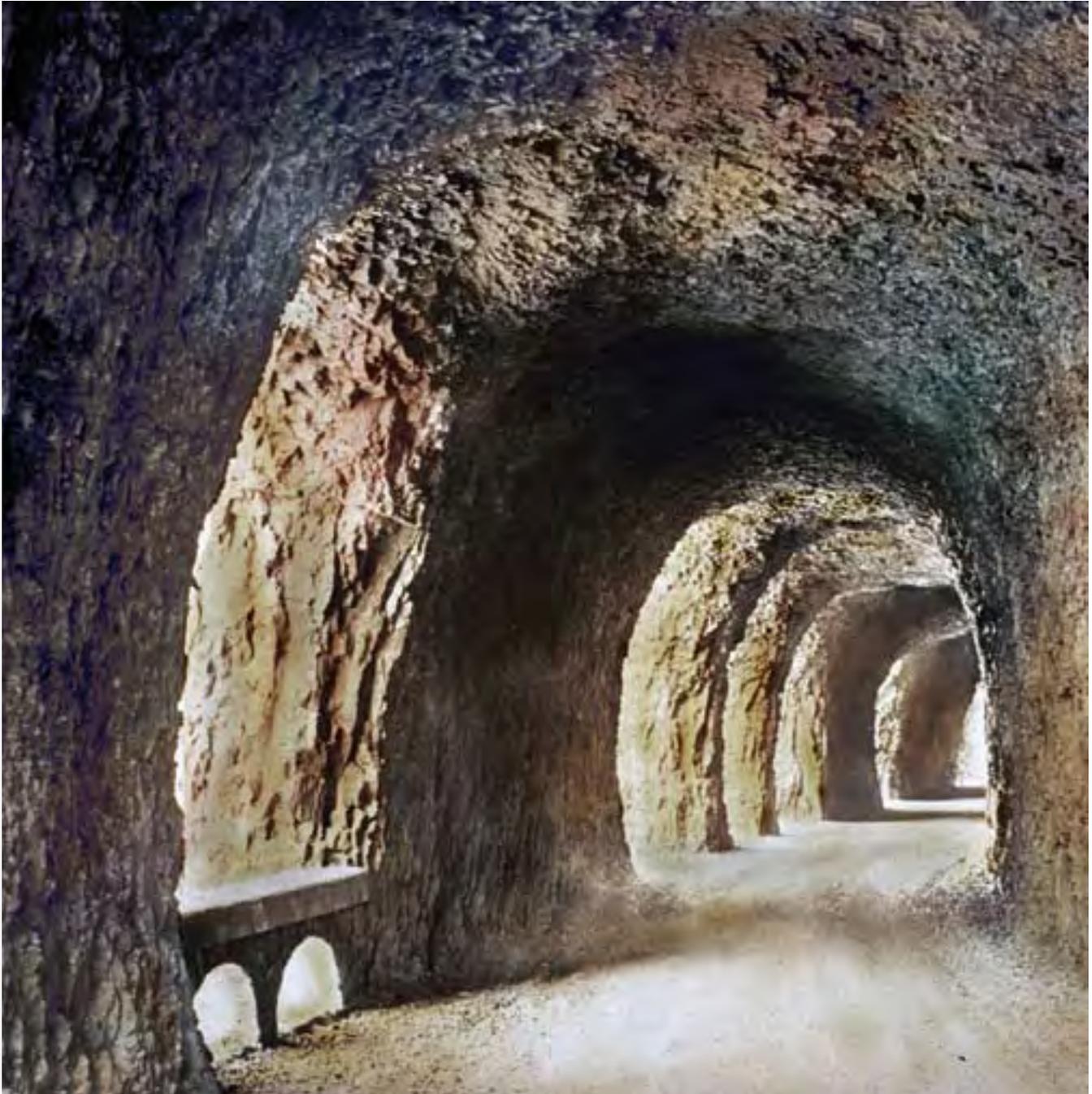


Figure 28—Mitchell Point Tunnel—The Tunnel of Many Vistas

j) Mitchell Point

Old mile post 61.1-61.3

Existing Conditions – Interpretive information at Mitchell Point tells visitors about the destroyed “Tunnel of Many Vistas.” The original tunnel location is a ledge, being used as a rockfall catch area.

Vision – Construct HCRH State Trail at Mitchell Point. Recreating the tunnel is proposed, because this proposal would be more likely to meet the visual requirements, as seen from Interstate 84. An additional, lower cost project is proposed to enhance the viewpoint, where the interpretive sign is located, with landscaping.

k) Mitchell Point to Hood River

Old mile post 61.3–63.6

Existing Condition – The rockwork at Ruthton Point has been restored, including the addition of an observatory and an interpretive sign.

Vision – Construct HCRH State Trail between Mitchell Point and Hood River, using as much of the original highway as possible. Ruthton Point should be connected to Hood River (near the Meredith Motel) and Mitchell Point, by constructing a bicycle facility along the northern slope of Interstate 84 to the frontage road undercrossing, and then utilizing the frontage road. This is the second highest priority project for the HCRH State Trail, after the Warrendale to Moffett Creek section.

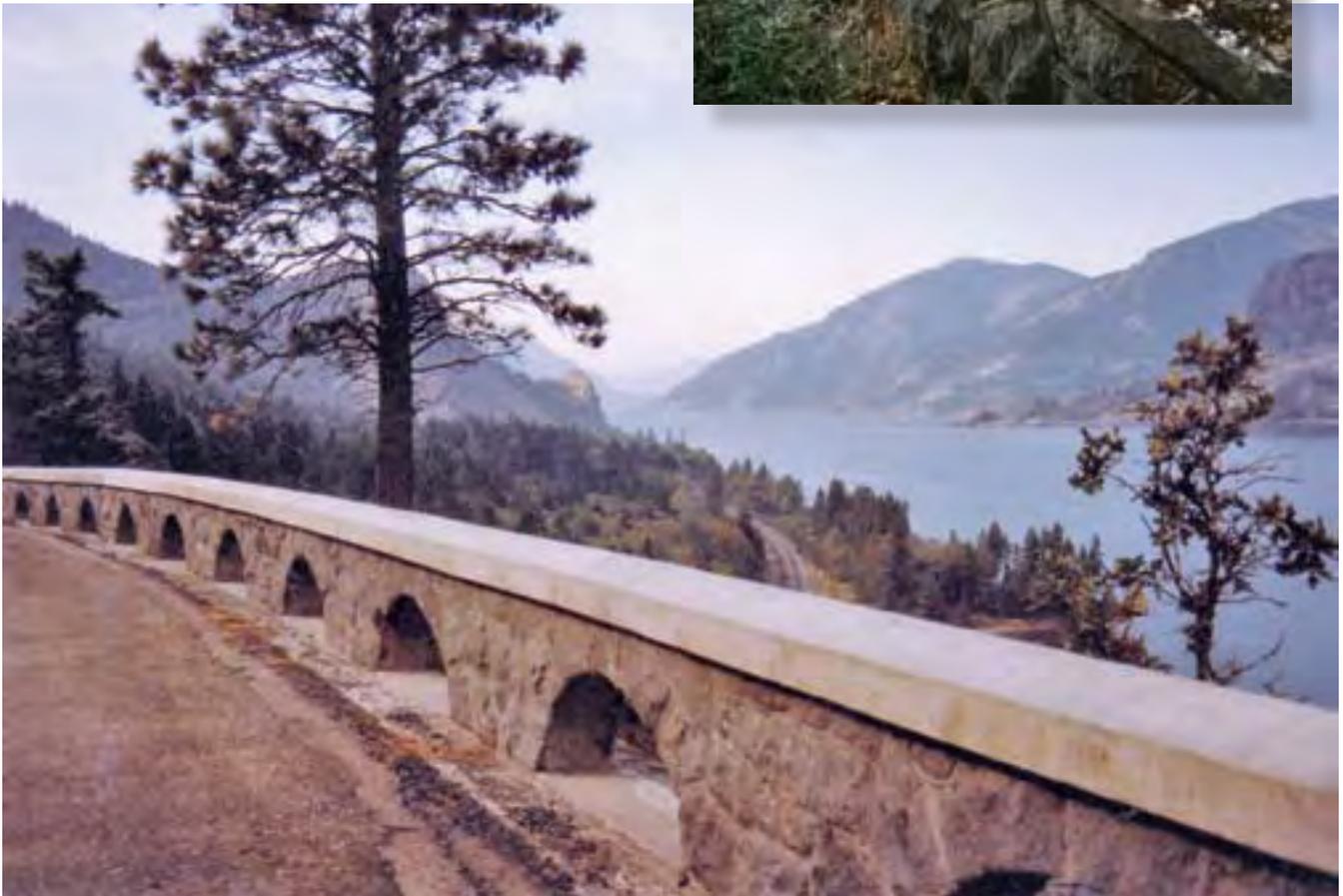


Figure 29—Ruthton Point Before (inset photo) and After Restoration.

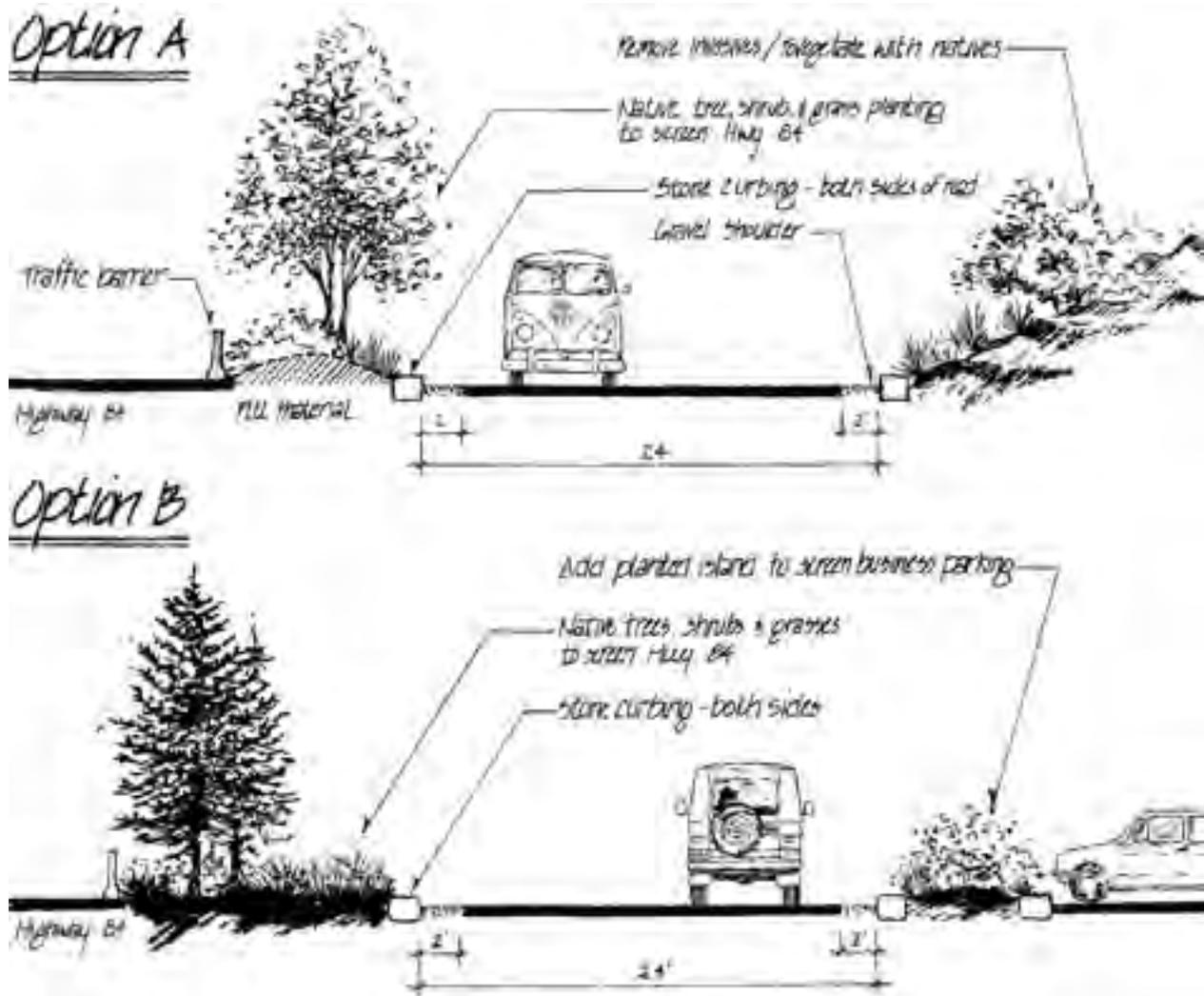


Figure 30—Proposed Enhancements for Westcliff Drive

l) Westcliff Drive

Old mile post 63.6–64.3

Existing Condition – This section partially follows the original alignment of the HCRH. There are currently several accesses that extend practically the whole length of a property.

Landscaping is minimal between Westcliff and Interstate 84.

Vision – The HCRH AC has proposed enhancements, as shown in Figure 30. These proposals need to be discussed with Hood River County, the owners of this facility.

m) Cascade Avenue

Mile posts 48.91–49.98

Old mile post 64.3–65.7

Existing Condition – The western end of this subsection is not distinguished. It is difficult to tie the Columbia Gorge Hotel to the HCRH south of the freeway. The highway has gravel shoulders near the freeway and curbs and gutters near Oak Street, and is lined with commercial properties. The lack of traffic controls gives this section a more rural feel than Oak Street. The recent addition of antique-style street lights encourages the historic feel of this area. Caps on street signs have recently been added to this section.



Figure 31—Pedestrian Railing in Hood River

n) Oak Street, Second Street and State Street

*Mile posts 49.98–51.26
Old mile post 65.7–66.8*

Existing Condition – The bustling commercial area retains its historic, small city charm. The City of Hood River has designated a downtown historic district that includes a portion of the HCRH. A committee has been formed to review building requests within the historic district. However, the downtown historic district has not been placed on the National Register of Historic Places.

Oak Street has sidewalks, curbs and gutters and several stop signs and one traffic signal. This is definitely a small city, with a residential area west of the commercial area.

The steep stairs rising on the south side of the highway are unusual and emphasize the topography of the city. The parallel parking and frequent crosswalks force traffic to slow down and enjoy the historic buildings. The City of Hood River proposes to place antique-style light fixtures through out the downtown area. Caps on street signs have recently been added to this area. A pedestrian railing was replaced with the Country Club Road to Oregon 35 project; any future railing replacements should be consistent with this railing.

Vision – Construct enhancements as detailed in the Programmatic Agreement.

o) Hood River to Senator Mark O. Hatfield West Trailhead

*Mile Posts 51.26–52.74
Old mile post 66.8–68.3*

Existing Condition – This section begins with the Hood River Loops, twisting and turning swiftly up the hillside. After the road straightens, it passes through a small residential area, with mail boxes on Highline Drive near the intersection with the highway. Near the gate the highway passes through a severely disturbed area, formerly used as gravel pits. A trailhead parking area



Figure 32—Hood River Loops

is located near the west side of the gate. The Twin Tunnels Visitor Station welcomes users with restrooms and interpretation. Art pieces have recently been added to the trail to the viewpoint overlooking the river. The parking area at Oregon 35 remains heavily used and will be enhanced with the HCRH Interpretive Sites and Signs project in 2006. Enhancements include two interpretive signs, paved parking and landscaping.

Vision – Maintain the HCRH. The Hood River Loops are expected to receive a pavement overlay by 2008.

p) Trailhead to Trailhead

Old mile post 68.3-73.1

Existing Condition – Noise from the freeway below is noticeable as you begin walking from the trailhead, but gradually becomes less noticeable as the distance and topography intervenes. Trees line the highway. Some features draw walkers for a closer look, including a rock wall, drinking fountain and old milepost 70. The former Hood River County East Pit has been reclaimed with native plants, including the endemic Barrett’s Penstemon, which flowers in the spring. A trail has been constructed on the east side of the pit with views to the Columbia River. The pavement has been returned to its original width, rock walls have been repaired and replaced, mileposts have been replaced and original dimension two-rail white guardrail has been restored.

Extensive restoration has occurred along the portion of the highway near the west portal of the Mosier Twin Tunnels. A 700-foot long rockfall catchment was constructed to prevent rocks from reaching the HCRH pavement or the Union Pacific Railroad below. Since the 1950s the Mosier Twin Tunnels were thoroughly filled with rock and the windows were closed with concrete blocks. The

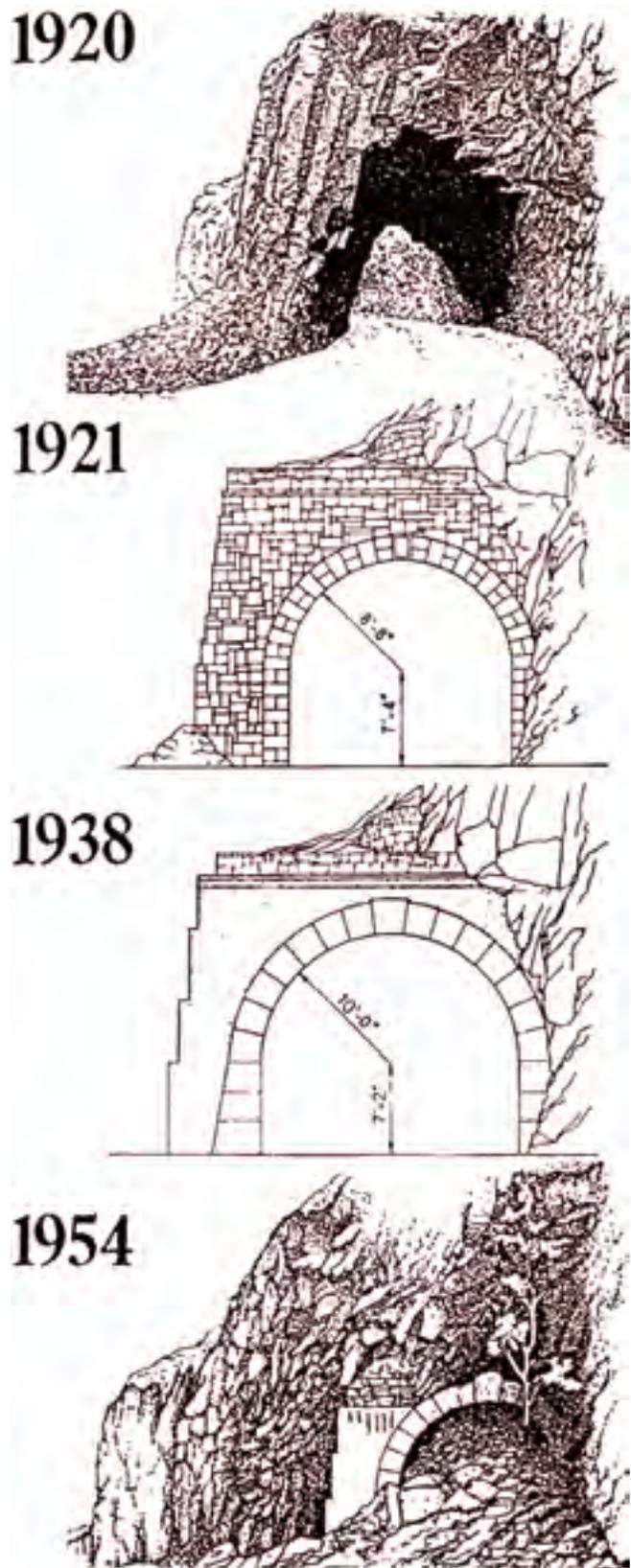


Figure 33—Evolution of West Portal of West Mosier Twin Tunnel

Tunnels have been reopened and restored to their 1920s appearance, with the exception of the east and west portals, which have been restored to their 1930s appearance. There is a noticeable change in vegetation from the west to the east side of the tunnels (heavy tree cover to open grassland). On the east side of the tunnels there is an expansive view up the Columbia River. The geology of the area is easily viewed. As the road turns south, it enters an extensive talus area. Intrusion into this area is restricted by a fence and screening vegetation.

Vision – Maintain the HCRH State Trail.

The CRGNSA *Management Plan* does not allow use of this section by horses.

q) Trailhead to Mosier

Existing Condition – A trailhead parking area has been developed near the junction of the HCRH and Rock Creek Road. Mile posts have been restored. A short section of cable and wood guardrail is in poor condition. The Rock Creek Bridge railing has been restored.



Figure 34—Restored Rubble Masonry Parapet Wall and Guardrail



Figure 35—New Overlook East of the Mosier Twin Tunnels is often called the Ellipse

Vision – Restore and maintain the HCRH, including replacement of guardrails with two-rail, steel-backed, wooden guardrail.

Recommendations

- 2.1 Connection projects should be planned and constructed to allow biking and hiking access between Warrendale and Moffett Creek, to complete the connection between the Frontage Road and WaNaPa in Cascade Locks. The facility will have a moderate level of wheelchair accessibility. Trails should use as much of the remaining pavement as possible. Additional interpretive signs will be installed in 2006 explaining the Tanner Creek area, Sheridan State Park and the mural on the Bridge of the Gods. Funding should be obtained for the connection projects. See priority listing under Funding.
- 2.2 The future cross-sections Cascade Locks will be those included in Figure 36 and Figure 37 (as included in the Programmatic Agreement, Appendix 10).

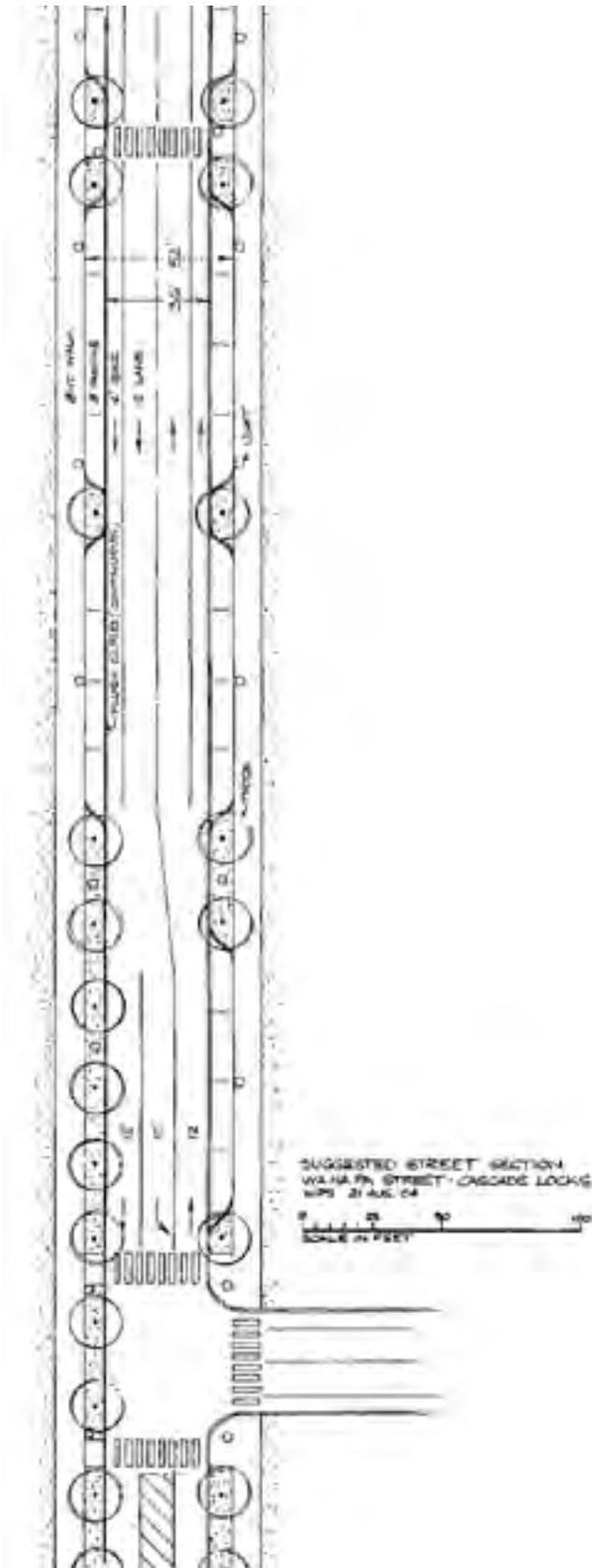


Figure 36—Typical Section for WaNaPa

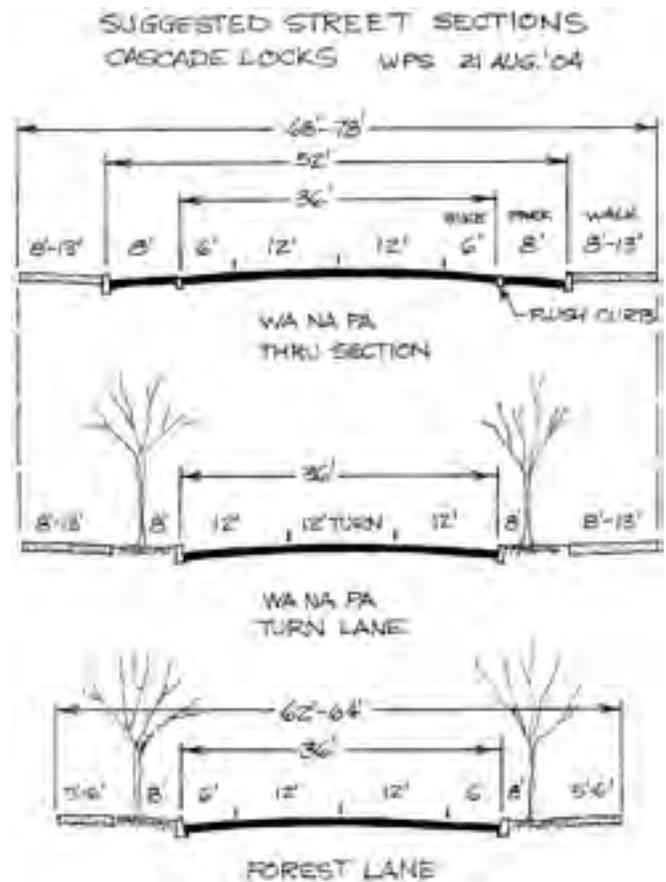


Figure 37—Cross Section for WaNaPa and Forest Lane

- 2.3 Future street lights in Cascade Locks will be determined by mutual agreement.
- 2.4 Off street parking in Cascade Locks should be encouraged, but on-street parking will remain.
- 2.5 Add interpretation about the HCRH at the intersection of WaNaPa and Forest Lane, within the proposed landscaped parking area. The City of Cascade Locks has proposed adding a restroom and visitor information building at this site in the future.
- 2.6 Add interpretation at the intersection of the HCRH and the access road to Marine Park.



Figure 38—View of the Original Hood River Bridge

- 2.7 Enhance the Mitchell Point viewpoint with additional landscaping and more attractive fence.
- 2.8 Install new restroom at Mitchell Point.
- 2.9 Add signing from I-84 to HCRH State Trail as part of trail projects.
- 2.10 Encourage City of Hood River to maintain the historic downtown area. Add antique-style light fixtures. Add bronze sidewalk plaques. Prohibit bulbouts on HCRH in Hood River.
- 2.11 Add interpretation at the Columbia Gorge Hotel and at the intersection of the HCRH and Oregon 35. Investigate other opportunities for a gateway and interpretation at the west end of Cascade Avenue (exit 62) in accordance with the Programmatic Agreement (Appendix 11).

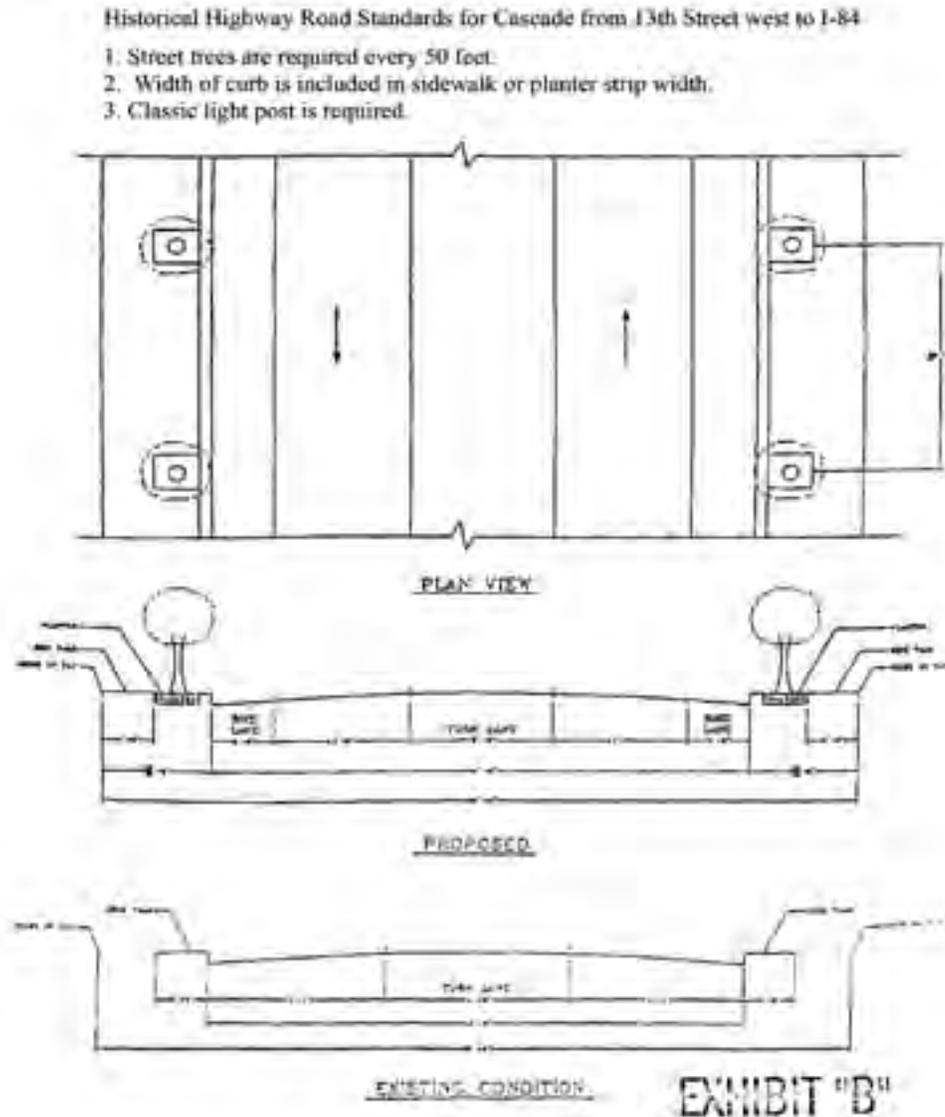


Figure 39—Cross-Section for Cascade Avenue

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| <p>2.9 Future cross-sections in Hood River will follow those outlined in the Programmatic Agreement (Appendix 11), as shown in Figure 39.</p> <p>2.10 Implement interpretive plan for the Twin Tunnels Visitor Station.</p> <p>2.11 Reactivate old drinking fountain between Hood River and Mosier, if potable water is available.</p> <p>2.12 Overlay pavement from Oregon 35 to the trailhead. Make improvements to bring this subsection up to same stan-</p> | <p>dards as other portions that are open to motor vehicle traffic.</p> <p>2.13 Improve the parking area at the intersection of Oregon 35 and the HCRH.</p> <p>2.14 A small section of guardrail needs to be replaced with two-rail, steel-backed, wooden guardrail, near Rock Creek.</p> <p>2.15 Seek expansion of the All-American Road designation to include all sections of the Highway in Hood River County, for a continuous route.</p> |
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